

**P/16/0517/FP**

MRS DIANE WALSH

**LOCKS HEATH**

AGENT: ANDREWS NEWBY  
PARTNERSHIP.

DEMOLITION OF EXISTING SINGLE STOREY 3 BED DWELLING AND ERECTION OF  
2NO TWO-STOREY 3 BED DWELLINGS AND ONE NO SINGLE STOREY 3 BED  
DWELLING.

19 ST JOHNS ROAD LOCKS HEATH FAREHAM SO31 6NE

***Report By***

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***Site Description***

The site currently contains a detached bungalow with a hipped roof and a detached garage to the north. The bungalow has a large garden which is significantly larger than those of neighbouring properties. The existing bungalow is positioned slightly closer to the front of the site than both of the adjacent properties. There are detached bungalows to the north and south of the site with a two storey, detached dwelling to the rear (west).

***Description of Proposal***

The application proposes to demolish the existing bungalow and garage and replace it with 2 number three bed, chalet style bungalows within the front half of the site, with 1 number two/three bed bungalow to the rear.

***Policies***

The following policies apply to this application:

National Planning Policy Framework 2012

Planning Policy Guidance

Fareham Borough Design Guidance (excluding Welborne) Supplementary Planning Document

Residential Car and Cycle Parking Standards Supplementary Planning Document

**Approved Fareham Borough Core Strategy**

CS2 - Housing Provision

CS4 - Green Infrastructure, Biodiversity and Geological Conservation

CS5 - Transport Strategy and Infrastructure

CS6 - The Development Strategy

CS9 - Development in Western Wards and Whiteley

CS15 - Sustainable Development and Climate Change

CS17 - High Quality Design

**Development Sites and Policies**

DPS1 - Sustainable Development

DSP2 - Environmental Impact

DSP3 - Impact on living conditions

DSP13 - Nature Conservation

DSP15 - Recreational Disturbance on the Solent Special Protection Areas

### ***Representations***

Objections have been received from 7 households in the area and 3 from further afield. The objections received raise the following issues:

Loss of privacy to number 23 from the bedroom window of plot number 2

Loss of privacy to 1A The Brackens

Disturbance in terms of noise and exhaust emissions from vehicle movements to 1A The Brackens and 23 St Johns Road

Noise pollution from additional families

Inappropriate density

The proposed development does not adhere to the existing building line along St Johns Road

Lack of detail regarding appearance and materials

Request trees and hedges are retained

Lack of parking

Impact of additional vehicles on the safety of St Johns Road

Overdevelopment

Loss of light and overshadowing of 1 The Brackens

The properties in The Brackens are at a lower height than the site, therefore they might be susceptible to problems regarding surface water drainage if the site is developed

Impact on wildlife

### ***Consultations***

Ecology: No objection subject to the implementation of the proposed mitigation and enhancement measures.

Highways: No objection subject to conditions.

Refuse and Recycling Team: No objection

### ***Planning Considerations - Key Issues***

Principle of development

The site is within the defined urban settlement boundary, therefore Policies CS2 and CS6 are applicable. In addition Policy CS9 which seeks to provide for residential development within the urban area provided that the setting of the area is protected, is also applicable.

The site comprises garden land which is no longer identified as previously developed land. Whilst this in itself is not a reason to resist development, proposals on residential garden sites must be considered against the criteria within Policy CS17 which requires all development to respond positively to and be respectful of key characteristics of the area including scale (amongst other criteria) form and spaciousness. The proposed addition of three dwellings to replace the existing dwelling is therefore acceptable in principle subject to satisfying the criteria of the Planning Policies summarized earlier in this report.

## Effect on the character of the area

St Johns Road is a residential road which is characterised by regularly spaced, detached properties with front gardens and on site parking. The majority of the dwellings within St Johns Road are either bungalows or chalet style bungalows. The land within St Johns Road gently slopes from the north down to the south. There is also a slight decrease in gradient from the east down to the west and as a result the properties in The Brackens (to the rear of the site) are at a lower level.

Policy CS17 requires all development to be of a high quality of design that responds positively to and is respectful of the key characteristics of the area, including (amongst other criteria) landscape, scale, form and spaciousness.

The 2 properties proposed in the front part of the site would be chalet style bungalows each containing 2 modestly proportioned, pitched roofed dormers on the front. There would be a 3 metre gap between number 1 and 2, with an 8 metre gap between number 1 and 17 and a 8.4 metre gap between number 2 and number 23. These separation distances are considered to be commensurate with the character of St Johns Road.

Concerns have been raised about the position of the chalet style bungalows as they would be positioned slightly forward of the neighbouring properties, however they would be positioned approximately in line with the existing dwelling and would be reasonably separated from the neighboring properties (as previously described). In addition there is a large garage directly south of the site, within the neighbour's front garden which also prevents the proposed position of the chalet style bungalows from being out of keeping with the existing character of St Johns Road.

There are a variety of roof forms within St Johns Road including pitched and hipped, therefore the proposed pitched roofs of the chalet style bungalows are considered to be appropriate.

The proposed bungalow would be positioned to the rear of the chalet style bungalows with parking to the side. Access to the proposed bungalow would be via a drive that would be positioned between plot number 2 and number 23. The proposed gap between number 2 and 23 would be a width of 8.4 metres, therefore there would be ample space for landscaping to be incorporated on either side of the drive.

The proposed bungalow would be set back from the front of the site by 32 metres. It would have a hipped roof and would be positioned to the rear of the chalet style bungalows. The bungalow would as a result only be visible from very limited vantage points within St Johns Road.

The proposed sub-division of the site to create three dwellings would result in garden sizes of a similar size to that of number 23 St Johns Road, to the immediate north of the site. Each of the proposed gardens would satisfy the minimum depth requirements recommended in the Council's Design Guidance (excluding Welborne) SPD.

It is considered that the proposed development would both respect and respond to the character of St Johns Road and would therefore be in accordance with the requirements of Policy CS17.

Concerns have been raised regarding the lack of detail regarding materials, however

appropriate materials can be secured by condition should Planning Permission be granted.

#### Effect on the neighbouring properties

There is one window in number 17's side elevation, however it is obscure glazed therefore the proposed chalet style bungalows would have a limited impact on number 17's amenities.

The proposed bungalow would be visible from within number 17's rear garden, however it would have a hipped roof and be set back from the boundary by 7 metres, therefore it would not have a significant impact on number 17's garden.

The proposed bungalow would also be visible from within the gardens of numbers 1, 1A and 2 The Brackens, however the hipped roof and position of the bungalow is such that it would not have a materially harmful impact on the neighbours' enjoyment of their gardens.

The occupiers of number 1 The Brackens have raised concerns about the impact of the proposed bungalow on their property as there is a bedroom/study window and a glazed back door (serving the kitchen) in the east elevation. The outlook from and amount of light available to the window in number 1's rear elevation is significantly compromised as it is located at a lower level than the site and currently fronts a close boarded fence. The applicant has responded to the neighbours' concerns by decreasing the size of the bungalow such that the part previously positioned directly opposite the bedroom/study window has been removed. The bungalow has also been repositioned to ensure a separation distance of 7 metres from the obscurely glazed kitchen door.

The occupiers of number 1A have raised concerns about loss of privacy, however there is a large leylandii hedge along number 1A's south boundary which provides a dense screen that would obscure views from the proposed bungalow into their garden. The proposed chalet style bungalows contain windows at first floor level in their rear elevations, however they would be separated by a distance of 23 metres from the nearest corner of number 1A's garden and would also only afford views at an oblique angle.

The occupiers of number 23 have also raised concerns about loss of privacy to their house and garden both from the rear dormers in the proposed chalet style bungalows at the front of the plot and from the drive which would provide access to the rear bungalow. Number 23 has a primary bedroom window, together with three further windows which serve an open plan living/dining room in their south elevation. The proposed drive would pass along the boundary with number 23, however it is not expected to result in a material loss of privacy as it would be separated from the boundary by 2 metres and from number 23 by 3 metres. In addition, there is an existing 1.8 meter high close boarded boundary fence along the full length of the site which would limit overlooking. The application also proposes soft landscaping along both sides of the drive which would prevent pedestrians from walking directly adjacent to the boundary fence.

Concerns have also been raised regarding the impact that the drive serving the rear bungalow would have on neighbouring properties in terms of noise pollution and fumes from cars. It is not considered that one household would generate sufficient vehicle movements as to result in material harm through levels of pollution. The drive could also be constructed of block paving which would result in less noise than the use of gravel or other loose surface materials.

Policy DSP3 states that development proposals should ensure that there will be no unacceptable adverse impact upon living conditions on the site or neighbouring sites. The

proposed development would increase the number of dwellings from one to three and would therefore result in an increased level of noise pollution, however it would not be significant enough to justify refusing the application. Overall the proposed development adheres to the separation distances recommended in the Council's Design Guidance (excluding Welborne) SPD and is considered to comply with the requirements of Policy DSP3.

#### Living conditions

The proposed development would provide 3 high quality dwellings each with on site parking and private amenity space that exceeds the minimum standards contained within the Council's Design Guidance (excluding Welborne) SPD.

#### Parking and highways

All three of the proposed dwellings incorporate on site car parking in line with the Council's Residential Parking Supplementary Planning Document. The bungalow positioned towards the rear of the site would also incorporate on site turning to enable vehicles to exit in a forwards gear.

Concerns have been raised regarding the impact that additional traffic movements would have on the highway. Whilst the proposed increase in the number of dwellings would result in additional vehicle movements into and out of the site, they would not be so significant that they would have an materially harmful impact on the safety of the road. The proposed development therefore complies with Policy CS5.

#### Ecology

Concerns have been raised regarding the impact the proposed development would have on wildlife and loss of habitat. The application is accompanied by an ecological statement containing measures to mitigate and enhance biodiversity within the site. The mitigation measures comply with the requirements of Policy DSP13 and can be secured by condition.

The applicant has provided the necessary financial contribution towards the Solent Recreation Mitigation Partnership interim strategy, such that the proposed development is considered to mitigate its impact and would, in combination with other developments, not increase the recreational pressure and habitat disturbance to the Solent Coastal Protection Areas in accordance with Policy DSP 15.

#### Conclusion

The proposed development, as amended is considered to be an acceptable form of development that is in accordance with the requirements of local and national planning policy and would not cause material harm to the visual amenities of the area, the street scene, highway safety or the living conditions of the occupiers of neighbouring properties.

The proposed development would make a more efficient use of the urban site and would provide two additional high quality dwellings which would contribute towards the Borough's need for housing.

#### **Recommendation**

PERMISSION, subject to conditions:

1. The development shall begin before the expiry of three years from the date of this decision notice.

REASON: To allow a reasonable time period for work to start, to comply with Section 91 of the Town and Country Planning Act 1990, and to enable the Council to review the position if a fresh application is made after that time.

2. The development shall be carried out in accordance with the following approved documents:

Site plan

Plots 1 & 2 elevations

Plot 3 elevations

St Johns Road elevation

Plot 2 (plot 1 handed) floor plans

Plot 3 bungalow ground floor plan

REASON: To avoid any doubt over what has been permitted.

3. No development shall take place above damp proof course level on any of the development hereby permitted until details of all materials to be used in the construction of the external surfaces of the development, have been submitted to and approved by the local planning authority in writing. The development shall be carried out in accordance with the approved details.

REASON: To secure the satisfactory appearance of the development.

4. Notwithstanding the details on the approved plans, no development above damp proof course shall take place until full details of soft and hard landscaping have been submitted to and approved in writing by the Local Planning Authority and these works shall be carried out as approved prior to first occupation or in the first available planting season following occupation. Any trees or plants which within a period of five years from first planting are removed, die or become seriously damaged or defective in the opinion of the local planning authority, shall be replaced within the next available planting season with others of the same species, size and number as originally approved.

REASON: To ensure the provision and establishment of landscaping.

5. None of the dwellings hereby permitted shall be occupied until the car parking and turning space for each respective dwelling have been laid out and provided for use in accordance with the details on the site layout plan. The designated areas shall thereafter be kept available and retained at all times for the purpose of parking.

REASON: In the interests of highway safety.

6. None of the dwellings hereby approved shall be occupied until details of secure cycle storage have been submitted to and approved in writing by the Local Planning Authority. All approved storage shall be installed and available for use prior to occupation of the dwelling it serves and shall thereafter be retained for the purpose of cycle storage.

REASON: In order to facilitate alternatives to the car.

7. None of the dwellings hereby approved shall be occupied until all 3 vehicular accesses, which will each incorporate visibility splays of 2 metres by 2 metres, have been constructed in accordance with the approved plans.

REASON: In the interests of highways safety.

8. No development shall take place until details of the measures to be taken to prevent spoil and mud being deposited on the public highway by vehicles leaving the site during the

construction works have been submitted to and approved by the local planning authority in writing. The approved measures shall be fully implemented upon the commencement of development and shall be retained for the duration of the construction of the development.  
REASON: In the interests of highway safety and the amenity of the area.

9. No work relating to the construction of any of the development hereby permitted (including works of demolition or preparation prior to operations) shall take place before the hours of 0800 or after 1800 Monday to Friday, before the hours of 0800 or after 1300 Saturdays or at all on Sundays, bank or recognised public holidays.  
REASON: To protect the amenities of the occupiers of nearby residential properties.

10. No materials obtained from site clearance or from construction works shall be burnt on the site.  
REASON: To protect the amenities of the nearby residents.

11. No development shall take place until details of how provision is to be made on site for the parking and turning of operatives vehicles and the areas to be used for the storage of building materials, plant, excavated materials and huts associated with the implementation of the permitted development have been submitted to and approved in writing by the Local Planning Authority . The areas and facilities approved in pursuance to this condition shall be made available before construction works commence on site (other than construction site access) and shall thereafter be kept available at all times during the construction period, unless otherwise agreed in writing with the local planning authority.  
REASON: In the interests of highway safety and the amenity of the area.

12. The mitigation and enhancement measures contained in the Ecological Survey (dated June 2015 and prepared by ecosupport ltd) shall be implemented in full, unless otherwise approved in writing by the Local Planning Authority. Thereafter, the enhancement measures shall be permanently maintained and retained in accordance with the approved details.  
REASON: To enhance biodiversity.

#### Informative

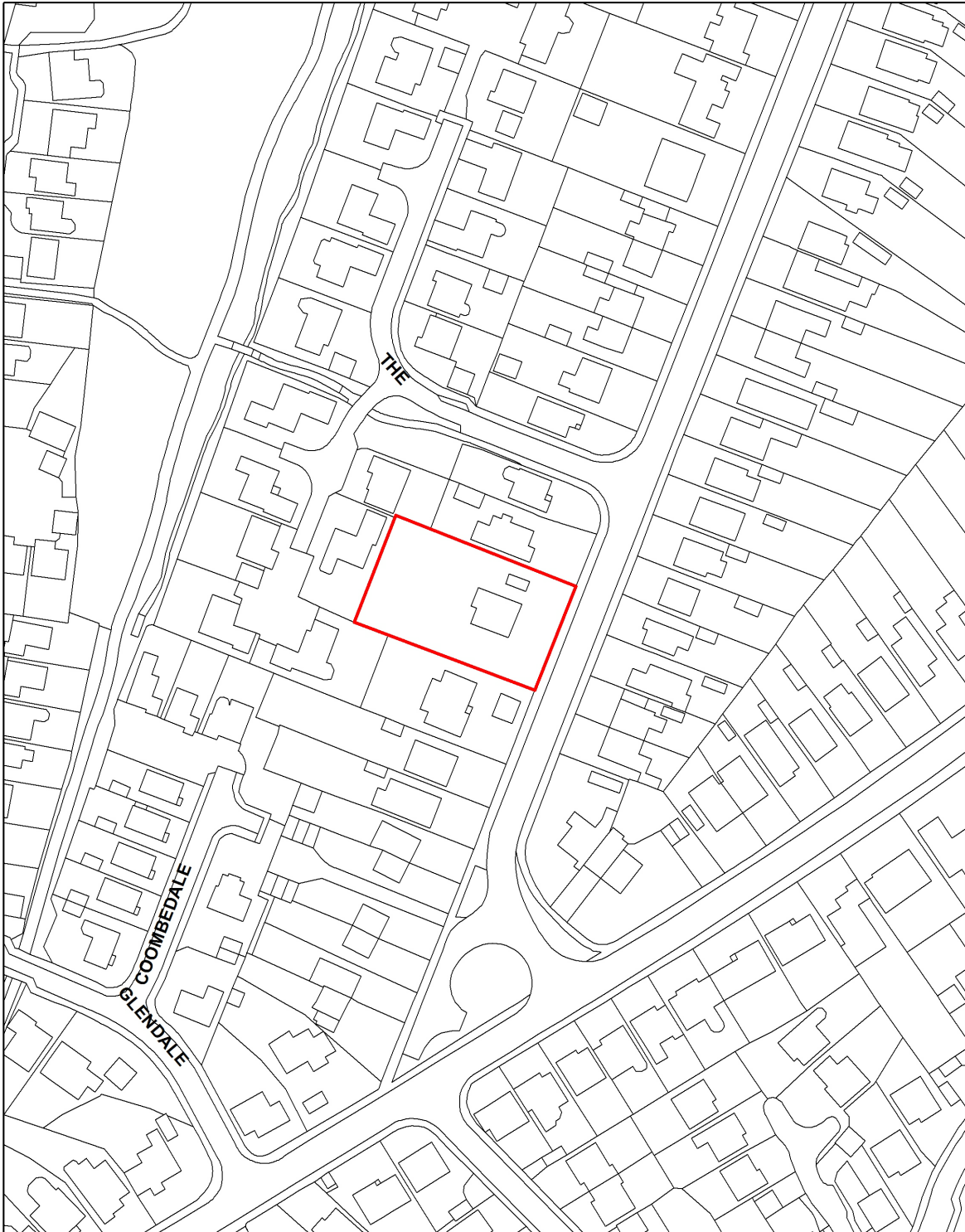
Bats and their roosts receive strict legal protection under the Wildlife and Countryside Act 1981 (as amended) and the Conservation of Habitats and Species Regulations 2010 (as amended). All work must stop immediately if bats, or evidence of bat presence (e.g. droppings, bat carcasses or insect remains), are encountered at any point during this development. Should this occur, further advice should be sought from Natural England and/or a L2 bat licensed professional ecologist.

#### ***Background Papers***

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# FAREHAM

BOROUGH COUNCIL



19 St Johns Road  
Scale 1:1250



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